

## **I. Columbia Ave**

The MOTI re-surfacing of Columbia Ave provides the city an opportunity to re-cast the downtown's surface treatments. In preparation, city staff conducted three design charrettes to solicit public input on alterations to the Columbia Ave corridor; the review took in the span from just west of the Miner's Hall to the corner of St. Paul and Columbia. The intent is to modify the Columbia Ave corridor to enhance its amenity appeal while improving its functionality as Rossland's commercial hub. This report recommends converting a portion of the asphalted right-of-way to pedestrian space to support commercial activities, rather than preserving it for backing out of parking stalls.

While Columbia Ave routinely serves Rossland as a commercial center and community gathering area during annual events, it also functions as Rossland's main transportation corridor. The Charrette exercise allowed the public an opportunity to comment on proposed alterations to the surface design in order to support an increase in sidewalk space, safer crosswalks, improved pedestrian flow and more commercial sidewalk activities.

Rossland's existing downtown layout limits its day-to-day pedestrian amenity and annual street festivities to two sidewalks, neither of which are wide enough to allow seating during activities and which provide very restrictive seating adjacent to restaurants or cafes. The generous width of the asphalted right-of-way currently allows diagonally parked cars to back out of their parking stalls while highway traffic continues flowing past them. This begs the question as to whether this is the best use of the space for the city's businesses and residents.

Between Spokane Ave and St. Paul Ave., the Columbia Ave right-of-way spans 100 feet/30.5 metres, of which a consistent 75 feet/ 22.8 metres are dedicated to a combination of transit lanes and parking stalls. However, once the distance for diagonal parking stalls (23 feet/ 7 metres) is subtracted, the transit space measures some 50 feet/15 metres, enough for four transit lanes; the minimum required width per lane is 3.65 metres. The re-surfacing project could allow the City the opportunity to re-design the Columbia Ave corridor that results in a reduced width for transit and an increased width for the north-side sidewalk.

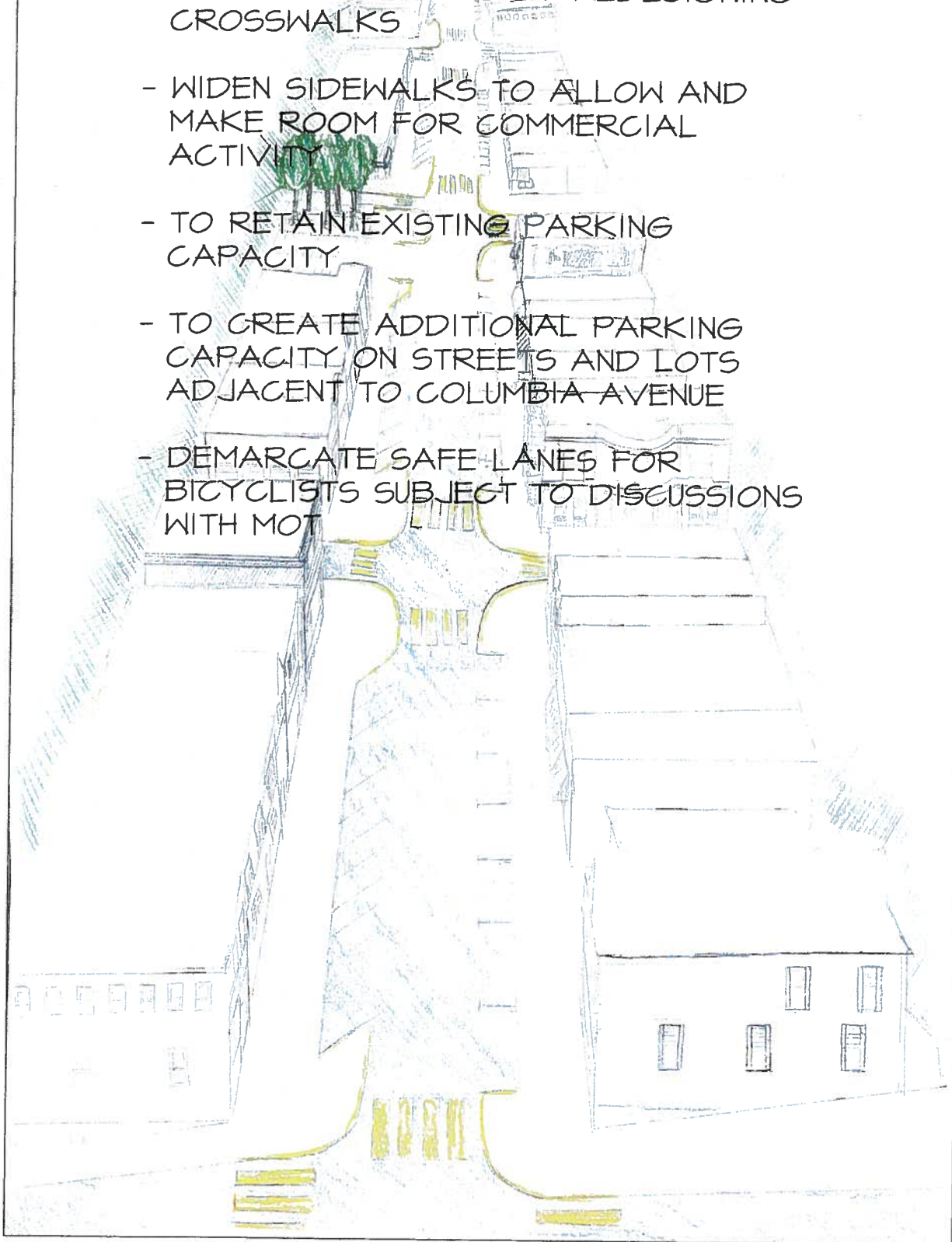
Ongoing discussions with MOTI will ultimately determine the exact dimensions of the Columbia Ave corridor layout. The initial drawings suggest that:

- The diagonal parking on the south side of Columbia Ave be retained, but re-angled to increase capacity on that side. Preliminary drawings suggest that re-angling the parking on the south side could increase the capacity within the 360 ft. long block (specifically between Washington and Queen St.) from 23 to 30 parking stalls.
- The north side parking between Spokane and St. Paul is suggested to be re-configured to parallel parking, featuring some 12 - 13 stalls per block, but only from late April to early October. The parking configuration would revert back to diagonal parking on or about October 1. This would require one permanent and one removable curb feature on the north side, and MOTI's approval for such a design.
- A separated bike lane is suggested to appear from late April to early October on the north side, to be located between the sidewalk curb and the parallel parking stalls. This feature would disappear in early October when the diagonal parking is re-created in order to ease parking in winter conditions.
- The suggested change to parking configuration during the warmer months would decrease parking capacity on Columbia Ave between Spokane and St. Paul by 2 -3 stalls per block. These would be replaced by an increase in stalls on Queen St. -- by striping new stalls on upper Queen & re-angling the stalls between NDCU and Ross Vegas -- adding new stalls on Spokane and re-angling all stalls on the south side.
- The Queen St. corridor south of Columbia Ave would be re-designed to increase its parking capacity through a mix of perpendicular and diagonal stalls.
- The parking area behind the NDCU would be improved to increase its capacity and make RV parking available in the downtown core. Le Roi Ave is suggested to be converted to a one-way leading to the highway in order to allow additional parallel parking on the north side.
- West of Spokane along Columbia Ave the pedestrian space (sidewalk) is suggested for re-design to safely accommodate both walking and cycling traffic to the Cliff St. right-of-way just beyond the Miners' Hall.
- Bump outs appear at the intersections of Spokane & Columbia, Washington & Columbia, and Queen & Columbia to facilitate shorter and safer crossing distances.

Charrette exercises prompted generally positive 84 responses to the suggestions for a re-design of the downtown layout. The most negative response was toward metered or paid parking while one of the most favourable responses appeared for more seating and benches in the downtown.

# OBJECTIVES:

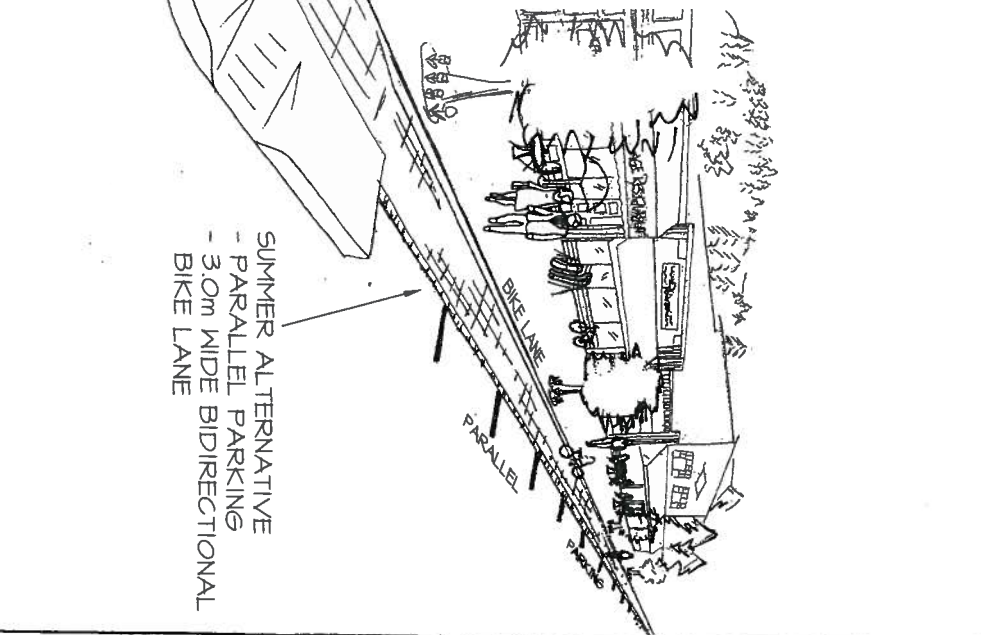
- INCREASE PEDESTRIAN ACCESS TO EXISTING SERVICES BY REDESIGNING CROSSWALKS
- WIDEN SIDEWALKS TO ALLOW AND MAKE ROOM FOR COMMERCIAL ACTIVITY
- TO RETAIN EXISTING PARKING CAPACITY
- TO CREATE ADDITIONAL PARKING CAPACITY ON STREETS AND LOTS ADJACENT TO COLUMBIA AVENUE
- DEMARCGATE SAFE LANES FOR BICYCLISTS SUBJECT TO DISCUSSIONS WITH MOT

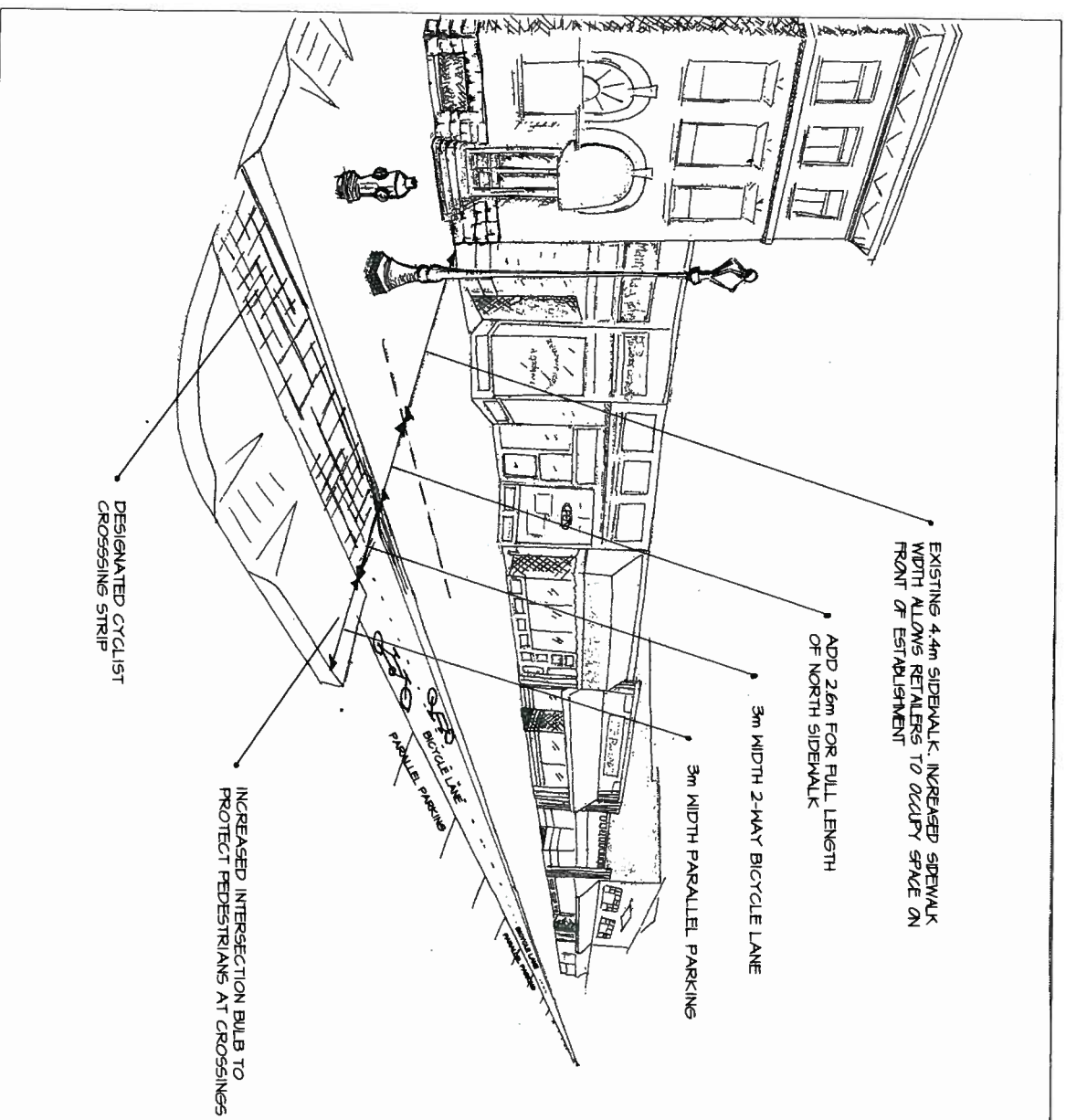


WINTER ALTERNATIVE  
- RETURN PARKING TO  
60° DIAGONAL



SUMMER ALTERNATIVE  
- PARALLEL PARKING  
- 3.0m WIDE BIDIRECTIONAL  
BIKE LANE





EXISTING 4.4m SIDEWALK. INCREASED SIDEWALK WIDTH ALLOWS RETAILERS TO OCCUPY SPACE ON FRONT OF ESTABLISHMENT

ADD 2.6m FOR FULL LENGTH OF NORTH SIDEWALK

3m WIDTH 2-WAY BICYCLE LANE

3m WIDTH PARALLEL PARKING

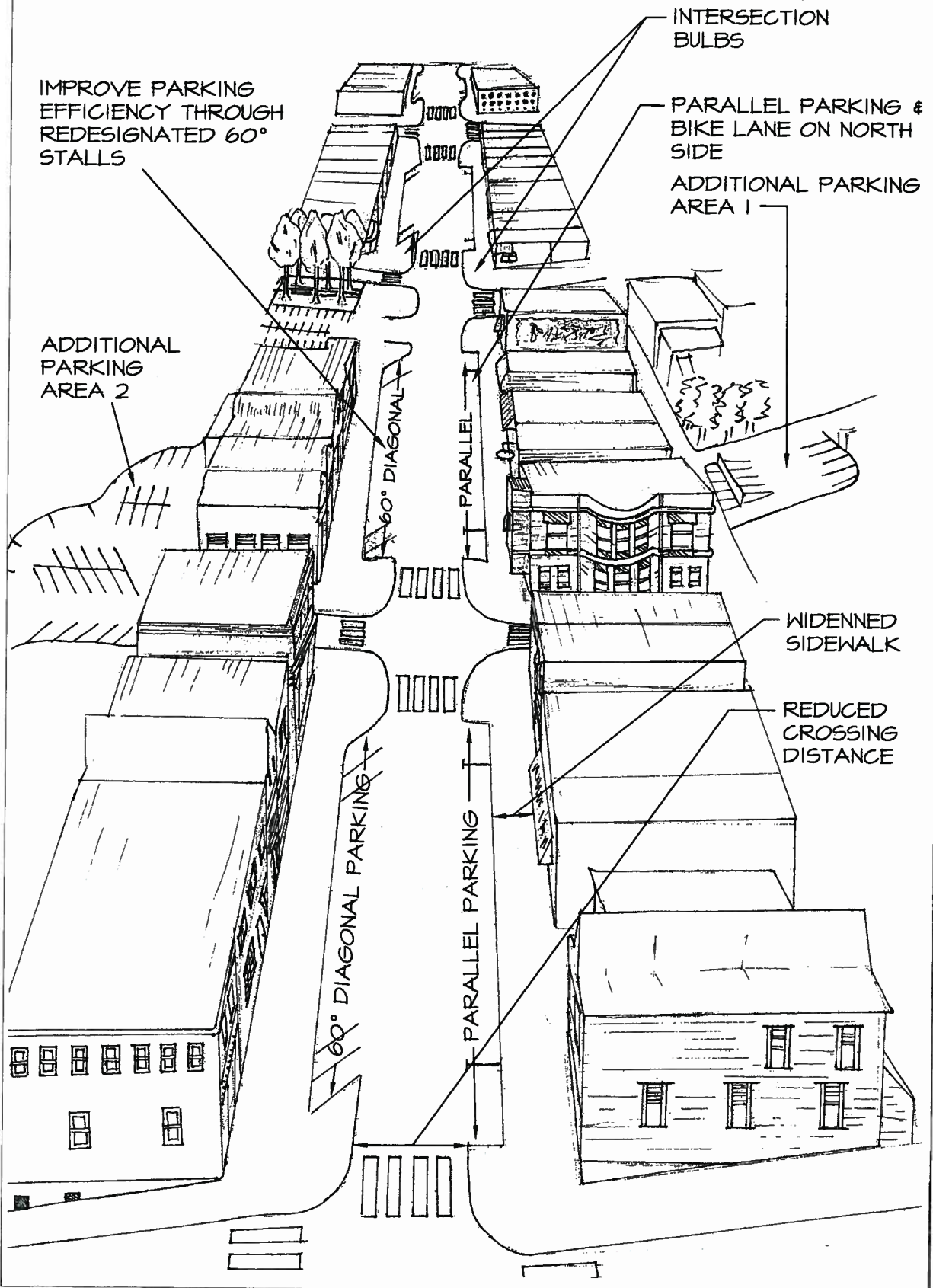
INCREASED INTERSECTION BULB TO PROTECT PEDESTRIANS AT CROSSINGS

DESIGNATED CYCLIST CROSSING STRIP

ROSSLAND  
COLUMBIA AVENUE  
OPTION 2.0



# OPTIONS





- AREA 3.0
- ADD SIDEWALK AND PARKING
  - 4 POTENTIAL STALLS

- AREA 1.0
- GRADE AND EXPAND PARKING
  - CONSTRUCT RETAINING WALL
  - 22 POTENTIAL STALLS (11 NEW STALLS)

- AREA 2.0
- REDEVELOP PARKING (26 STALLS)
  - DESIGNATE LEROI AVE. ONE-WAY

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2246 Columbia Ave. Castlegar, B.C. V1N 3X1 Ph: (250) 365-5696

**CITY OF ROSSLAND**  
**COLUMBIA AVENUE**  
**INFRASTRUCTURE UPGRADE**  
**POTENTIAL PARKING**  
**DEVELOPMENT AREAS**

SCALE: NOT TO SCALE  
 JOB: C10118-009  
 DATE: 23/11/2010  
 FIGURE: 1

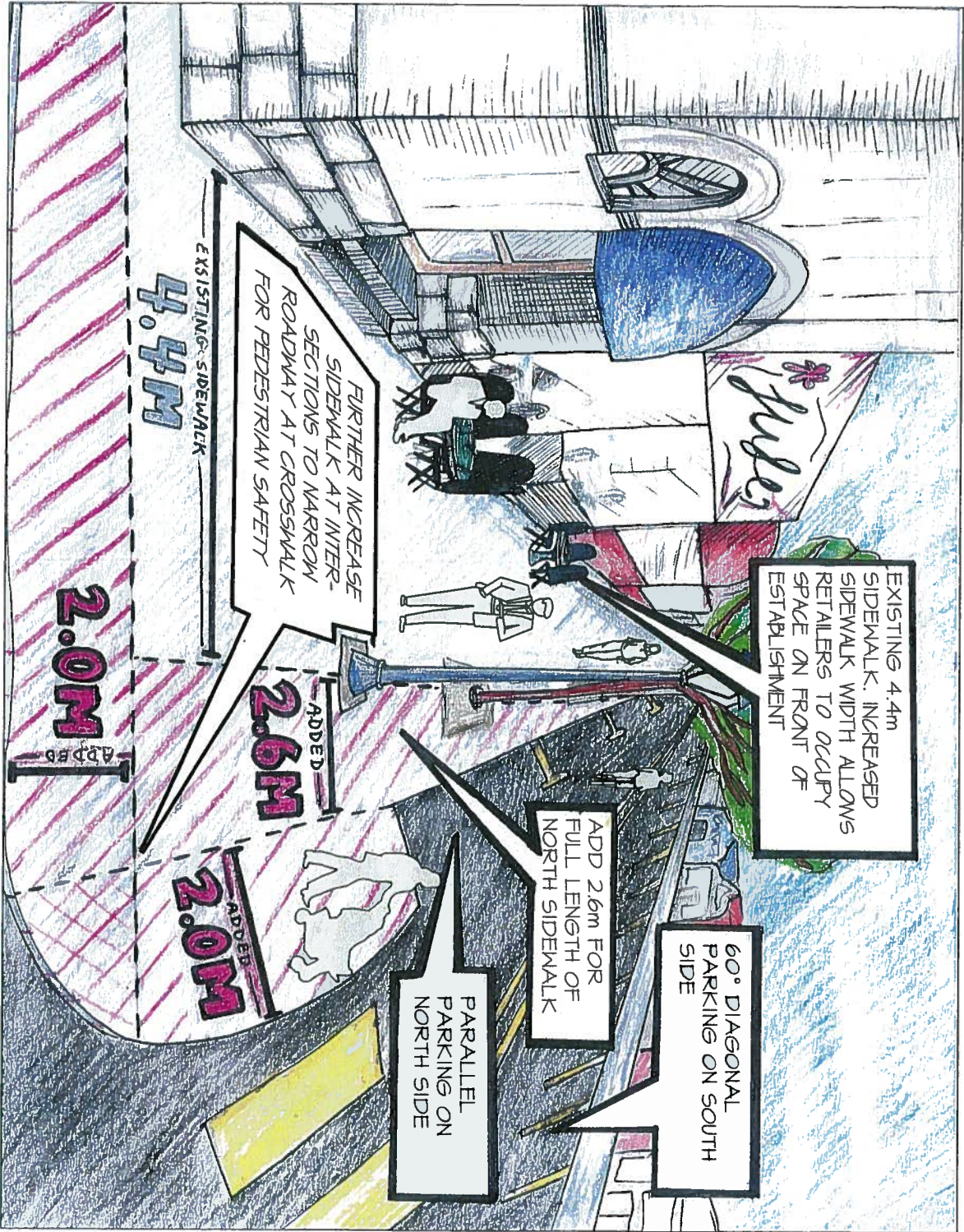


ORIGINAL 22.6m

PROPOSED 8.5 TO 11.5m

### INTERSECTION PEDESTRIAN BULBS

- REDUCE CROSSING DISTANCE BY AS MUCH AS 10 METERS
- OPTIONS FOR ACCENT PAVERS OR STAMPED CONCRETE
- USE BULB SPACE TO REDUCE OBSTACLES ON THE MAIN WALKWAY



EXISTING 4.4m SIDEWALK. INCREASED SIDEWALK WIDTH ALLOWS RETAILERS TO OCCUPY SPACE ON FRONT OF ESTABLISHMENT

FURTHER INCREASE SIDEWALK AT INTERSECTIONS TO NARROW ROADWAY AT CROSSWALK FOR PEDESTRIAN SAFETY

4.4M

EXISTING SIDEWALK

2.0M

ADDED

2.6M

ADDED

2.0M

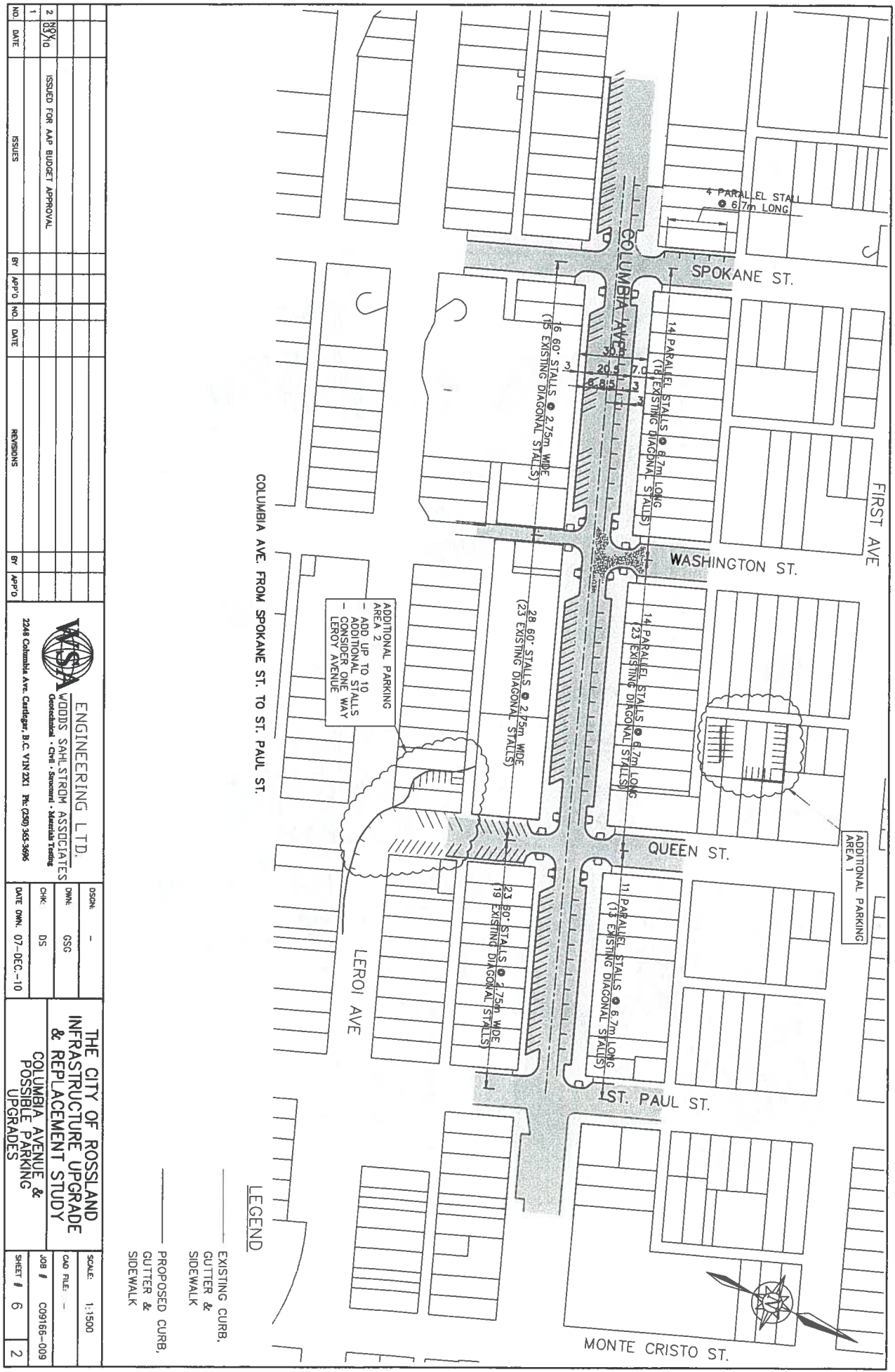
ADDED

60° DIAGONAL PARKING ON SOUTH SIDE

ADD 2.6m FOR FULL LENGTH OF NORTH SIDEWALK

PARALLEL PARKING ON NORTH SIDE

Jules



NO.	DATE	ISSUES	BY	APPROV'D	NO.	DATE	REVISIONS	BY	APPROV'D
2	08/10	ISSUED FOR A44 BUDGET APPROVAL							
1									

<p><b>WISA ENGINEERING LTD.</b> WOODS SAH STROM ASSOCIATES Consulting • Civil • Structural • Mechanical • Electrical</p> <p>2248 Columbia Ave. Cambridge, B.C. V1N 2X1 Tel: (250) 365-9696</p>	<p>OSDN: -</p> <p>DMN: GSG</p> <p>CHK: DS</p> <p>DATE DMN: 07-DEC.-10</p>	<p>THE CITY OF ROSSLAND INFRASTRUCTURE UPGRADE &amp; REPLACEMENT STUDY COLUMBIA AVENUE &amp; POSSIBLE PARKING UPGRADES</p>	<p>SCALE: 1:1500</p> <p>CAD FILE: -</p> <p>JOB #: C09166-009</p> <p>SHEET # 6 2</p>
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