

**11.0 OFFSTREET PARKING AND LOADING**

**11.1 Application of Regulations**

Space for the off-street parking and loading of motor vehicles in respect of a use permitted under this Bylaw shall be provided and maintained in accordance with the regulations of this section.

**11.2 Exemptions from Parking and Loading Requirements**

The regulations contained in this section shall not apply with respect to land, a **Building** or a **Structure** existing at the date of adoption of this Bylaw, so long as the land, **Building** or **Structure** continues to be put to a use that does not require more parking or loading spaces than were required for the use existing at the date of adoption of this Bylaw.

**11.3 Calculating the Number of Required Parking and Loading Spaces**

The number of required parking and loading spaces shall be calculated as follows:

1. The number of off-street parking spaces required for a use, **Building** or **Structure** shall be calculated according to the Off-street **Parking** Table 11.1 of this section.

**Diagram 11.1 | Required Off-street Parking Spaces**

COLUMN I CLASS OF BUILDING	COLUMN II REQUIRED NUMBER OF PARKING SPACES
<i>RESIDENTIAL DWELLINGS</i>	
One and Two <b>Family Detached Dwellings</b>	2 per principal dwelling unit
	1 per principal dwelling unit if each principal dwelling unit has a net floor area less than 70 sq. m.
One <b>Family Detached</b> with <b>Secondary Suite</b> or <b>Detached Secondary Suite</b>	2 per principal dwelling unit, plus 1 per <b>Accessory</b> dwelling unit
	1 per principal dwelling unit if principal dwelling unit has a net floor area less than 70 sq. m
<b>Multiple Conversion</b> dwelling	2 per principal dwelling unit
	1 per principal dwelling unit if each principal dwelling unit has a net floor area less than 70 sq. m.
<b>Guest House</b>	1 per sleeping unit

<p><b>Mobile Home</b></p> <p><b>Multiple Family Dwellings</b> including Apartment Dwellings, Apartment Dwellings in Conjunction with a Commercial Use and Townhouse Dwellings</p> <p>Bed &amp; Breakfast</p>	<p>1 per dwelling unit</p> <p>a) 1.0 off-street parking stall per dwelling unit with one or two sleeping units;</p> <p>b) 1.5 off-street parking stalls per dwelling unit with three sleeping units;</p> <p>c) 2.0 off-street parking stalls per dwelling unit with 4 or more sleeping units;</p> <p>plus 0.2 per dwelling unit for visitor parking</p> <p>2 per dwelling unit plus 1 per sleeping unit</p> <p>1 per principal dwelling unit if each principal dwelling unit has a net floor area less than 70 sq. m.</p>
<p><i>COMMERCIAL</i></p> <p>Art Studio</p> <p>Automotive or vehicle sales</p> <p>Automotive or vehicle repair</p> <p>Broadcasting Studios</p> <p>Building and garden supply</p> <p><b>Financial Institutions</b></p> <p>Funeral Home</p> <p>General <b>Office</b></p> <p><b>Golf Course</b></p> <p><b>Hotel</b></p> <p><b>Motel</b></p> <p>Medical and Dental <b>Clinics</b></p> <p>Neighbourhood Oriented Store</p> <p>Nurseries, Greenhouses</p> <p><b>Personal Service Establishments</b></p> <p><b>Restaurants, Cafes, Public Houses, Clubs or Lodges</b></p> <p><b>Retail Stores</b></p> <p style="padding-left: 20px;">GFA less than 1000 sq. m.</p> <p style="padding-left: 20px;">GFA 1000 sq. m. to 2000 sq. m.</p> <p style="padding-left: 20px;">GFA 2000 sq. m. to 20,000 sq. m.</p> <p style="padding-left: 20px;">GFA greater then 20,000 sq. m</p> <p>Service stations, vehicle washes</p>	<p>2.0 per 100 sq. m. GFA</p> <p>2.0 per 100 sq. m. GFA</p> <p>2 per service bay</p> <p>2.5 per 100 sq. m. GFA</p> <p>1 per 37 sq. m. of commercial floor area</p> <p>2.5 per 100 sq. m. GFA</p> <p>1 per 5 seats</p> <p>2.5 per 100 sq. m. GFA</p> <p>3 per hole</p> <p>1 per 2 guest rooms</p> <p>1 per guest room</p> <p>1 per 50 sq. m. of gross floor area</p> <p>2 per 100 sq. m. of retail floor area</p> <p>1 per 30 sq. m.</p> <p>1 per 45 sq. m. of gross floor area</p> <p>1 per 6 seats</p> <p>2.0 per 100 sq. m. GFA</p> <p>2.3 per 100 sq. m. GFA</p> <p>2.4 per 100 sq. m. GFA</p> <p>2.5 per 100 sq. m. GFA</p> <p>1 per 2 employees on duty, plus 1 per service bay, plus additional required spaces for other associated uses (e.g. retail)</p>

Ski Resort	1 per 4 units of Comfortable Carrying Capacity (As determined by the Canadian Alpine Ski Policy Guidelines.)
<b>Theatres</b>	1 per 4 seats
Unspecified Commercial Uses	1 per 30 sq. m of gross floor area
<i>INDUSTRIAL</i>	
All light industrial uses on all parcels	1 per 50 sq. m. of gross floor area
<i>PUBLIC AND INSTITUTIONAL</i>	
Community Halls and <b>Churches</b>	1 per 3 seats
Daycare	1 per employee on duty
School	1 per classroom
<b>Hospital</b>	1 per employee per shift plus 1 per 5 beds
Intermediate Care Facility	1 per 2 employees plus 1 per 3 beds
Libraries, Civic and Public Service <b>Buildings</b>	1 per 90 sq. m.
Nursing and Extended Care Homes	1 per employee on shift
Recreation Facility	1 per 45 sq. m. gross floor area plus 1 per 10 seats
Stadium or Arena	1 per 4 seats
Unspecified <b>Institutional</b>	1 per 50 sq. m of gross floor area

2. Where the calculation of the required off-street parking spaces results in a fraction, the required number of stalls shall be rounded down to the next full number.
3. Where seating is the basis for determining the number of off-street parking spaces and seating consists of benches, pews, booths or similar seating arrangements, each 0.5 m of width of such seating shall be deemed to be one seat.
4. In the case of mixed uses, the total requirements for off-street parking shall be the sum of the requirements for the various uses computed separately.
5. Notwithstanding 11.13 (4), cumulative parking requirements for mixed-use occupancies may be reduced where it can be determined that the peak requirements of the occupancies occur at different and not conflicting times, subject to the following conditions:

The applicant shall submit a parking demand summary sheet identifying the greatest shared parking demand for all uses by using the demand calculations providing in Table 11.2

**Parking Occupancy Rates.** The calculations shall be completed as follows:

- a) The minimum number of parking spaces to be provided for each use shall be determined by using Table 11.1 Required Off-street **Parking Spaces**.

- b) The minimum number of parking spaces for each use shall be multiplied by the "occupancy rate" for each weekday and weekend time period.
- c) For each time period, determine the combined total number of parking spaces required for all uses.
- d) Identify the greatest minimum number of parking spaces required amongst the various time periods to establish the period of greatest shared demand
- e) Once the greatest shared parking demand for all weekday and weekend time periods is determined, Council will take into account the following factors in consideration of any reduction in parking requirements:
  - 1) Distance between sharing uses and the parking facility;
  - 2) Pedestrian connections among sharing uses and the parking facility;
  - 3) Vehicular connections;
  - 4) Whether parking will be paid;
  - 5) Location and proximity to the town centre area;
  - 6) General surrounding development density;
  - 7) Proximity to transit corridors or stations;
  - 8) Special trip reduction programs, such as vanpooling, transit, shuttle or telecommuting;
  - 9) Need for any reserved parking spaces.

**Table 11.2 |** Parking Occupancy Rates

Uses	M-F	M-F	M-F	Sat. & Sun.	Sat. & Sun.	Sat. & Sun.
	8am-5pm	6pm-12am	12am-6am	8am-5pm	6pm-12am	12am-6am
Residential	60%	100%	100%	80%	100%	100%
<b>Office/ Warehouse/Industrial</b>	100%	20%	5%	5%	5%	5%
Commercial	90%	80%	5%	100%	70%	5%
<b>Hotel</b>	70%	100%	100%	70%	100%	100%
<b>Restaurant</b>	70%	100%	10%	70%	100%	20%
<b>Movie Theatre</b>	40%	80%	10%	80%	100%	10%
Entertainment	40%	100%	10%	80%	100%	50%
Conference/Convention	100%	100%	5%	100%	100%	5%
<b>Institutional</b> (non-church)	100%	20%	5%	10%	10%	5%
<b>Institutional</b> (church)	10%	5%	5%	100%	50%	5%

*Note: This table defines the percent of the basic minimum needed during each time period for shared parking. (M-F = Monday to Friday); Source: VTPI 2008*

- f) The number of on-street parking stalls adjacent to land, a **Building** or a **Structure** in the C1 zone shall be credited against the number of required off-street parking spaces.
- g) Developments may not exceed minimum parking requirements beyond a maximum of 20%.

#### **11.4 Access and Circulation**

- a) Each off-street parking or loading space shall be accessible by vehicle from a highway.
- b) Where more than 10 off-street parking and loading spaces are provided, access to these spaces from an aisle which intersects with a highway shall be provided.
- c) Where more than 30 off-street parking spaces are provided, access to these spaces from an aisle that intersects with a highway at least at two points shall be provided.
- d) Access driveways shall:
  - 1) Have a maximum permitted width at the point of access of 4.0 m
  - 2) Not exceed a 20% grade.
  - 3) Be limited to one for parcels with a frontage of 20 m or less.
  - 4) Wherever possible, be shared where adjoining access driveways occur on adjacent parcels.

#### **11.5 Payment of Money in Lieu of Parking Spaces**

- a) With the exception of parcels, **Buildings** or a **Structures** located in an residential zone, payment of money instead of providing required off-street parking spaces may be made, at the option of the owner or occupier of the land, **Building** or **Structure**, to the City of Rossland when the City of Rossland determines compliance otherwise with these regulations would be undesirable or impractical.
- b) Where payment of money instead of providing required off-street parking spaces is made it shall be in the amount of \$3,000.00 per off-street parking space, subject to annual review, which amount shall be placed in the City's parking facilities reserve fund.

#### **11.6 Surfacing of Off-Street Parking and Loading Spaces**

- 1) Except for areas of adverse soil conditions as determined by the **Building Inspector**, 40% of the total parking area required by this bylaw shall be permeable or semi permeable surface.
- a) The installation of permeable surfaces shall:
  - 1) Follow manufacturer specifications for minimum and maximum slopes.

- 2) Include the construction of a sub-grade to allow adequate drainage and prevent frost heave.
  - 3) Ensure drainage shall not directly discharge onto neighbouring lands.
  - 4) Include the installation of perforated sub-drains below permeable surfaces, as required, to store, filter or convey water to additional stormwater facilities.
  - 5) If permeable surfaces are planned for use along driveways leading to public streets or other heavily traveled routes, the surface material and base course must be selected, designed and certified to withstand the anticipated traffic loading stresses and maintenance impacts.
- b) Landscaped islands, containing trees, shall be provided every 4 (four) vehicle surface spaces within a surface parking area.

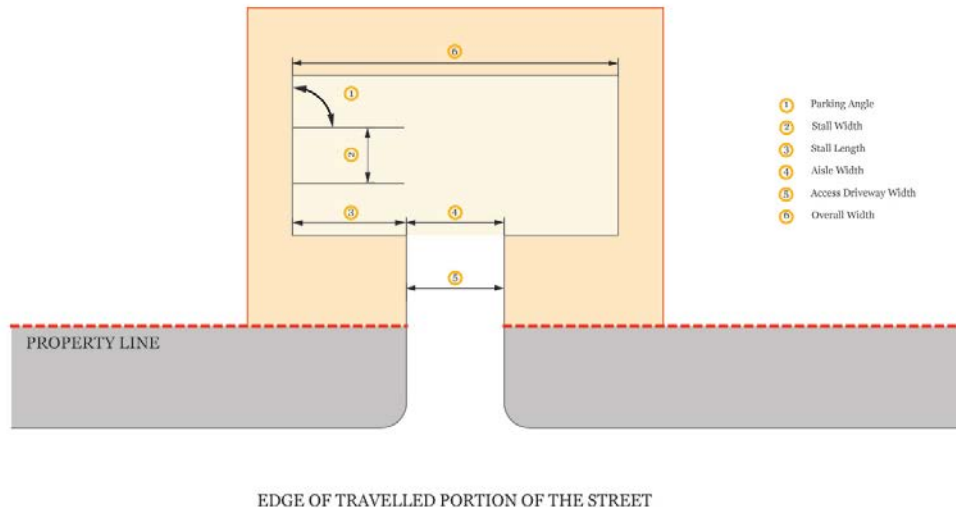
### 11.7 Dimension of Parking Spaces

a) In **Zones** other than **R Zones**, each off-street parking space and parking lot shall be provided in accordance with the following minimum specifications:

**Diagram 11.3** | Specifications for Parking Stalls and Structures

Parking Angle	90°	75°	60°	45°	Parallel
Stall Width	2.9m	2.9m	2.9m	2.9m	2.9m
Stall Length	5.5m	5.5m	5.5m	5.5m	6.5m
<b>Aisle</b> Width	6.0m	6.0m	5.0m	4.0m	5.0m
Access Driveway Width	4.0m	4.0m	4.0m	4.0m	4.0m

b) In R-zones the minimum parking space shall be 3.0m wide by 5.0m long



### 11.8 Location of Parking Spaces

- a) In all residential **Zones**, required off-street parking spaces shall be provided and maintained on the same land as the use, **Building** or **Structure** they serve.
- b) In **Zones** other than residential **Zones**, off-street parking spaces may be located on a site not more than 70 metres from the site on which the use, **Building** or **Structure** is located, provided that the owner enters into a restrictive covenant, approved by City **Council** and registered on title, providing the use of the site for parking in conjunction with the site on which the use, **Building** or **Structure** is located.

### 11.9 Parking for Visitors of Multi-Family and Mixed Use Dwellings

- a) Visitor parking shall be identified by a sign with the words “VISITOR PARKING”, and shall be at a location that is safe, convenient and easily found.
- b) Pedestrian access to visitor parking areas and from the visitor parking area to the main lobby or an individual residential unit shall be adequately identified with signage that is clear in its directions to the visitor.

### 11.10 Parking for Disabled Persons

- a) **Parking** for the use of disabled persons shall be provided in the compliance with the requirements of the *British Columbia Building Code* and successor codes or, in the event such requirements do not apply, where more than 20 parking stalls are required every off-street parking facility shall provide 1% of the required stalls, with a minimum of 1 stall, for the use of disabled persons. Each such stall shall:
  - 1) be at least 4.0 m in width and at least 6.0 m in length;

- 2) be located as close as possible to a main disabled-accessible **Building** entrance;
- 3) be clearly identified for the exclusive use of disabled persons.

### **11.11 Snow Removal**

For parcels containing 4 or more parking spaces, the following regulations shall apply:

- a) If the parking area is required to be used in the winter an additional area equal to 20% of the required parking area shall be provided for snow storage on-site. Specific areas to be cleared shall include the full dimensions of roadways, walkways, and parking areas and aisles.
- b) Areas required for snow storage may not be counted towards parking requirements.
- c) A 50% reduction in the required snow storage area shall be permitted if an adequate snow melt system is constructed for any parking area.
- d) A 70% reduction in the required snow storage area shall be permitted if solar technology is used for the snow melt system constructed for any parking area.
- e) These snow storage and melt elements shall be designed so that snow is not stored in a manner where, when melting, it directly discharges into riparian areas or other public drainage ways or in any way harms the quality of the City's water. Snow shall not be placed in any creek or other public drainage ways within the City.
- f) Provide snow storage areas away from public streets and other areas where motorist and pedestrian sight lines are essential.
- g) Sod areas, portions of landscaped areas, bio-retention areas and overflow parking areas may be identified for snow storage with any plant material selected accordingly.
- h) Permeable surfaces are encouraged for use in snow storage areas.
- i) The owner or occupier of the site shall, at its cost, make arrangements to remove the snow from the parking area to a location acceptable to the City of Rossland in a timely manner and no later than 24 hours after a snowfall.
- j) Snow storage is only permitted on public rights-of-way subject to the approval of the Director of Public Works and where the distance from the edge of the paved and municipally-maintained roadway to the front or side property line is in excess of 4m.

### **11.12 Other Regulations**

- a) Where a dwelling unit or units are located in a non-residential zone in conjunction with a non-residential use, on-site parking must be located in such a way as not to impair access to the premises for the non-residential use.
- b) No on-street parking is permitted within the Red Mountain Resort Village Area.

**11.13 Driveway**

- a) In a residential zone, off-street parking shall have access from a street or lane. The driveway shall have a minimum width of 2.4 m but not to exceed 4.0 m and shall be graded and surfaced pursuant to the surfacing requirements for parking stated in section 11.

**11.14 Loading Space Area**

- a) No use may be undertaken in any zone unless the off-street loading requirements in this Bylaw have been met for that use.
- b) The number of off-street loading spaces required for any use is calculated according to Table 11.3 of this Bylaw:

**Diagram 11.3 | Required Off-Street Loading Spaces**

COLUMN I	COLUMN II
Retail store, Manufacturing, Fabricating, Processing, Warehousing, and Wholesaling Establishment	
(i) less than 300 sq. m. in floor area	0
(ii) 300 to 465 sq. m. in floor area	1
(iii) 465 to 2,300 sq. m. in floor area	2
(iv) greater than 2,300 sq. m. in floor area	3

- c) In cases of mixed use, required loading spaces for one use shall not be considered as required loading spaces for any other use, unless it can be determined that the peak loading requirements of the various uses occur at different and not conflicting times, subject to the terms and conditions set out in Section 11.
- d) Off-street parking spaces shall not be credited as off-street loading spaces.
- e) Each off-street loading space shall be not less than 7.5m in length, 3.0 m in width and shall provide no less than 4.0 m of vertical clearance. In no case shall the length of a loading space be such that a vehicle in the process of loading or unloading shall extend into any highway.
- f) Off-street loading facilities shall be located on the same site as the use, **Building or Structure** served, but not within the required front or side **setback** or closer than 7.5 m to the nearest intersection of any two highways.
- g) Each off-street loading space shall have vehicular access to a highway.
- h) Off-street loading space surfaces shall be subject to Section 11.
- i) Where illumination of off-street loading areas is provided, lighting fixtures shall be located, arranged and shielded so that no direct rays of light are oriented upwards or shine beyond the boundaries of the **Parcel**.

- j) Off-street loading spaces shall be sited at an elevation convenient to a floor level used for loading purposes in the **Building**.
- k) Off-street loading spaces existing on the adoption date of this Bylaw shall not be reduced below the applicable requirements for off-street loading in this Bylaw.

#### **11.15 Storage of Vehicles and Materials**

- a) No commercial vehicle; disassembled or wrecked vehicle; trailer; recreational trailer or vehicle; construction or heavy equipment of any kind or any **Building** material shall be parked or stored in an R zone or CD zone except as follows:
  - 1) one commercial vehicle not exceeding 4,000 kg Gross Vehicle Weight (GVW) or manufacturer's rated capacity of one (1) tonne;
  - 2) one recreational vehicle or trailer per unit;
  - 3) commercial vehicles or equipment required for the construction, repair, servicing or maintenance of the premises during regular working hours;
  - 4) **Building** materials when the owner, lessee or occupier of the premises is in possession of a valid **Building** permit, provided that the materials stored are in connection with the construction or development of the **Building** pursuant to the **Building** permit.
- b) No site in a zone other than an industrial zone shall be used for:
  - 1) the storage of junk or for the wrecking of a motor vehicle;
  - 2) the storage of a motor vehicle which has been without a license under the Motor Vehicle Act for a period of more than one year and is not housed in a garage or carport, and which is intended to be self-propelled but is not capable of moving under its own power.

#### **11.16 Bicycle Parking**

- a) For a parcel in a multi-family residential zone or a commercial **Zones** containing three or more residential dwelling units, bicycle parking facilities shall be provided capable of accommodating the parking of that number of bicycles determined on the following basis:
  - 2 bicycle parking spaces per unit;
- b) Where bicycle parking is required, at least 50% of the bicycle parking spaces provided shall be located in a weather-protected area close to the main entrance of the **Building** for which the bicycle spaces are intended.
- c) Payment of money instead of providing the required bicycle parking spaces may be made, at the option of the owner or occupier of the parcel, **Building**

or **Structure**, to the City of Rossland when the City of Rossland determines compliance otherwise with these regulations would be undesirable or impractical.

- d) Where payment of money instead of providing the required bicycling parking space is made it shall be in the amount of \$125 per bicycle space, which amount shall be placed in the City's parking facilities reserve fund.